

1 **Table of Contents**

2 **ARTICLE XVI. GATEWAY CORRIDOR OVERLAY DISTRICT..... 2**  
3 SECTION 16.1 ESTABLISHMENT .....2  
4 SECTION 16.2 INTENT.....2  
5 SECTION 16.3 APPLICABILITY .....2  
6 SECTION 16.4 DESIGNATED GATEWAY CORRIDORS: .....3  
7 SECTION 16.5 ALLOWABLE USES .....3  
8 SECTION 16.6 DEFINITIONS: .....4  
9 SECTION 16.7 REQUIRED DRAWINGS:.....5  
10 SECTION 16.8 BULK STANDARDS: .....5  
11 SECTION 16.9 BUILDING TYPES.....6  
12 SECTION 16.10 REQUIRED LANDSCAPING AND BUFFERING ..... 14  
13 SECTION 16.11 SITE DESIGN STANDARDS ..... 18  
14 SECTION 16.12 SIGNS; NONCONFORMING SIGNS ..... 23  
15 SECTION 16.13 UTILITY LINES AND DEVICES..... 25  
16 SECTION 16.14 WALLS AND FENCES ..... 25  
17 SECTION 16.15 PARKING OF COMMERCIAL AND RECREATIONAL VEHICLES..... 26  
18 SECTION 16.16 SCREENING OF MECHANICAL UNITS ..... 26  
19 SECTION 16.17 STORAGE/SERVICE AREAS; MISCELLANEOUS..... 26  
20 SEC. 16.18 EXTERNAL SITE LIGHTING ..... 27  
21 SECTION 16.19 VARIANCES/SPECIAL EXCEPTIONS..... 28  
22 SECTION 16.20 PENALTIES ..... 28  
23 SECTION 16.21 ARCHITECTURAL STYLE ..... 28  
24  
25  
26

1

## 2 **ARTICLE XVI. GATEWAY CORRIDOR OVERLAY DISTRICT**

### 3 **Section 16.1 Establishment**

4 In addition, and supplemental to, all land development code requirements heretofore and hereafter,  
5 there is hereby created an overlay zoning classification known as the "Gateway Corridor Overlay  
6 Classification."

### 7 **Section 16.2 Intent**

8 The intent of this article is to ensure that the designated Oakland Gateway Corridors are developed  
9 in a manner that:

- 10 A. Insures subject roadways are developed into well landscaped scenic gateways;
- 11 B. Provides uniform design standards to establish high-quality development;
- 12 C. Maximizes traffic circulation functions from the standpoint of safety, roadway capacity, multi-  
13 modal integration, vehicular and non-vehicular movement, and minimizes curb cuts along  
14 arterial roadways;
- 15 D. Encourages a mix of uses that provides for multi-modal connectivity and eco-tourism  
16 opportunities;
- 17 E. Preserves tree canopy, environmental assets and natural features to the extent practical; and
- 18 F. Recognizes and makes allowances for existing uses and buildings.

### 19 **Section 16.3 Applicability**

20 Provisions of this article are applicable in all commercial, industrial, and planned development  
21 zoning districts inclusive of the R-N-C district, within those areas designated in Section 16.4.  
22 Residential structures are not subject to this ordinance. Additions to existing church facilities,  
23 including new structures on existing church sites, shall not be subject to the architectural design  
24 criteria of this ordinance, provided the proposed structure conforms to an architecturally  
25 recognizable design, and said design is consistent with the existing facilities. Standards stated  
26 herein shall be in addition to, but take precedence over, standards found elsewhere in the Zoning  
27 Code of the Town of Oakland. This ordinance shall apply to all new development, and also to the  
28 following:

29 *Renovations and redevelopment:* In the case of additions or renovations to, or redevelopment of,  
30 an existing building or project, where the cost of such addition, renovation, or redevelopment  
31 exceeds 25 percent of the taxable assessed value of the existing structures, or 20 percent of the  
32 square footage of the existing structures, the provisions of this ordinance shall apply.

33 *Discontinuance:* Where the use of a structure ceases for any reason for a period of more than 365  
34 consecutive days, except where governmental action impedes access to the premises, the  
35 provisions of this ordinance, excluding setback requirements, which may require structural  
36 alterations shall be adhered to prior to re-occupancy. With respect to vehicular use and required  
37 landscape areas, the provisions of this ordinance shall apply where the use of a structure ceases

1 for any reason for a period of 180 consecutive days, except where governmental action impedes  
2 access to the premises.

3 *Conversions:* Where residential structures are converted to businesses within an R-N-C district,  
4 and said structure was built within the last 2 years, all provisions of this ordinance shall apply.  
5 Where the structure was built greater than 2 years prior, the Development Review Committee shall  
6 determine the extent of retro-fitting necessary to accommodate the intent of this ordinance.

7 **Section 16.4 Designated Gateway Corridors:**

8 A. Old Town Corridor

9 Those lands within 200 feet, north and south, of the adopted centerline of Oakland Avenue (State  
10 Road 438) from the Town limit on the east, to the Orange County / Lake County line on the west.  
11 Additionally, all lands within 200 feet, east and west, of the centerline of Tubb Street, from its  
12 intersection with State Road 50 on the south, to Briley Avenue on the north. If any part of the parcel  
13 abuts the right-of-way line of the designated roadway, the entire parcel shall be subject to this  
14 chapter as if the parcel were within the stated corridor width.

15 B. Highway 50 Activity Corridor

16 Those lands within 320 feet, north and south, of the adopted centerline of State Road 50 from the  
17 Orange County / Lake County line on the west to the Town limit on the east. If any part of a parcel  
18 abuts the right-of-way line of the designated roadway, the entire parcel shall be subject to this  
19 chapter as if the parcel were within the stated corridor width.

20 **Section 16.5 Allowable Uses**

21 A. Purpose. The purpose of this section is to strengthen and diversify the commercial activities  
22 within the key corridors of the Town of Oakland.

23 B. Uses. To create and encourage a healthy, vital marketplace, and provide for a complementary  
24 mix of uses along Highway 50 which will generate services to the Town of Oakland residents  
25 and professionals, while maintaining a profitable business environment, the following  
26 limitations for certain uses will apply to the underlying zoning districts within the Gateway  
27 Corridors:

28 1. Any new or expansion of the following existing uses within the Gateway Corridors shall  
29 apply for a Special Exception::

- 30 a. 1. Vehicle related sales/services;
- 31 b. Eating and Drinking facilities with drive through services;
- 32 c. Eating and Drinking facilities that serve alcoholic beverages;
- 33 d. Convenience stores (including gasoline sales); and
- 34 e. Houses of worship.

35 Applications for special exceptions shall be determined through a consideration of the  
36 specific location of the property within the Gateway Corridor and adjacent uses. Such  
37 applications will be granted only if the proposed use complements or coordinates with  
38 adjacent uses, diversifies the economic base of the area, and comports with the intent of  
39 the Gateway Corridor ordinance.

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38

C. Applicability. The applicability of these regulations shall survive the subdivision of land. All land subject to these regulations on the date of adoption of this ordinance shall continue to be subject to these regulations subsequent to subdivision, even if said subdivision creates a separate lot located greater than 320 feet from the centerline of Highway 50, or greater than 200 feet from the centerline of Oakland Avenue .

**Section 16.6 Definitions:**

For the purpose of this chapter the following definitions shall apply unless the context clearly indicates or requires a different meaning.

APPEARANCE REVIEW BOARD. That board of the Town created pursuant to Article V, Section 5.9 of the Town Code. Decisions made by the Appearance Review Board are recommendations to the Town’s Planning and Zoning Board, which board makes recommendations to the Town Commission, which commission makes final decisions on the part of the Town.

BUILDING LOT TYPE, Establishes basic parameters governing building form, including the envelope for building lot requirements (setbacks, building lot area size, frontage building), permitted height, and the location for parking in relationship to the building.

BANNER SIGNS. Any signs having characters, letters, illustrations, or ornamentations applied to cloth, paper, or fabric of any kind.

LANDSCAPE BUFFER. That area immediately abutting the right-of-way line to which landscaping standards apply.

GROUND SIGNS. Signs wholly independent of any building for support.

OFF-PREMISES SIGNS. Any sign whose copy, logo or other sign graphics do not relate to goods, services or activities offered on the property to which the sign is attached. A billboard is considered to be an off-premises sign.

POLE SIGNS. A sign supported by poles, uprights, or braces, permanently placed on or in the ground and wholly independent of any building for support, either single or double faced,

- PORTABLE SIGNS. Any sign that is designed to be transported including but not limited to signs:
- 1. with wheels removed;
  - 2. with chassis or support constructed without wheels;
  - 3. designed to be transported by trailer or wheels;
  - 4. converted to an A or T-frame signs;
  - 5. attached temporarily or permanently to the ground, structure, or other signs;
  - 6. mounted on a vehicle for advertising purposes, parked, and visible from the public right-of-way, except signs identifying the related business when the vehicle is being used in the normal day-to-day operations of that business;
  - 7. menu and sandwich boards;

1 8. searchlight stands; and

2 9. air or gas-filled balloons, or umbrellas used for advertising.

3 SHADE TREES. Shall mean specifically Live Oaks (*Quercus virginiana*), four inch diameter at  
4 breast height (dbh) at the time of planting.

5 TEMPORARY SIGNS. Any sign, not including flags, which is not permanently affixed to a building  
6 or structure, which is constructed of cloth, canvas, light fabric, cardboard, wallboard, or other light  
7 materials, with or without flames. Snipe signs are considered to be temporary signs.

8 TOWN PLANNER. That Town employee or Town consultant whose duties include addressing  
9 issues relating to planning, zoning and development within the Town.

10 TOWN TRANSPORTATION ENGINEER. That Town employee or Town consultant whose duties  
11 include addressing issues relating to transportation issues within the Town.

12 UNDERSTORY TREES. Shall mean any of the following, as long as they are a minimum of two inches (dbh) at  
13 time of planting: Loquat (*Eriobotrya japonica*), Savannah Holly (*Ilex opaca* 'Savannah'), East  
14 Palatka Holly (*Ilex opaca* 'East Palatka'), Drake Elm (*Ulmus parvifolia* 'Drake'); or any of the  
15 following multi-trunk trees, provided they have three to four one inch trunks: Cape plumbago  
16 (*Plumbago auriculata*), Crape Myrtle (*Lagerstroemia indica*), or Tree Ligustrum (*Ligustrum*  
17 *lucidum*).

18 WALL SIGNS. Any sign painted on, or attached to, or erected parallel to the face of; or erected  
19 and confined within the limits of the outside wall of any building and supported by the wall or  
20 building and which displays only one advertising surface.

21 UTILITY LINES. Utility lines of all kinds, including, but not limited to, those franchised utilities,  
22 electric power and light, telephone and telegraph, cable television, water, sewer, and gas.

23 **Section 16.7 Required Drawings:**

24 The Town Planner will provide the Town's Sufficiency Check list which outlines all the required  
25 documents that are required prior to the Town Planner review of any application. Architectural  
26 elevations of all facades of all structures subject to this Ordinance shall be a required exhibit for  
27 development plan approval as outlined in Article 20, Sec. 2 in the Town's Land Development Code.  
28 Such exhibits shall include colors, materials, building dimensions, location of service areas and  
29 mechanical equipment, screening devices, parking, site furnishings, lighting fixtures, all signage,  
30 and any other information as determined necessary to ensure consistency with the intent of this  
31 Ordinance by the Town. All elevations must be signed and sealed by a licensed architect  
32 registered in the State of Florida.

33 **Section 16.8 Bulk Standards:**

34 The following table outlines the permitted bulk standards, by gateway corridors.

Table 16.8.1 Gateway Corridor Overall Bulk Standards		
Bulk Standards (1)	Highway 50 Activity Corridor	Old Town Corridor
Front Setback (2)	Contingent on utility easement need (2)	10' min- 20' maximum

<b>Option 1</b>	10' min- 25' max (refer to option 2 landscape front buffer requirements)	N/A
<b>Option 2</b>	40' minimum (refer to option 1 landscape front buffer)	N/A
<b>Side Setback</b>	Refer to Building Lot Types	
<b>Rear Setback</b>	25' when adjacent to residential parcels (RNC-20% of lot depth or 25' whichever is greater)  0' when adjacent to non-residential parcels	
<b>Height</b>	35', additional height may be requested through the Special Exception Process	
<b>Density</b>	(Refer to underlying zoning district)	
<b>Floor Area Ratio (FAR)(3)</b>	.35/.25 (C-1, C-2, I-1, I-2) .35 RNC	
<b>Impervious Surface Ratio (ISR) (3)</b>	.65 for RNC, .75 all other zoning districts	

- 1 (1) Setback requirements are for all non-residential development. If a parcel develops  
2 under I-1, I-2, or A-1, the underlying bulk standards may apply as requested.
- 3 (2) Only landscape, hardscape, signage or parking may be located within the front  
4 setback. No portion of the building, canopies, drive-thru may be located in the  
5 setbacks. When required, the setback may be modified, by the Town Planner, for a  
6 utility easement as outlined in Sec. 16.9 Required Landscape Standards.
- 7 (3) FAR and ISR may be increased as a special exception for infill lots only.

### 8 Section 16.9 Building Types

9 Building types are permitted within the Gateway Corridor Overlay District when new  
10 and/or redevelopment occurs on any parcel. The use of the building typologies is to be  
11 consistent with the size, scale and character desired within this Overlay District as  
12 identified in Table 16.9.1 Permitted Building Types by Area.

Table 16.9.1 Permitted Building Types by Area		
	<b>Highway 50 Corridor</b>	<b>Old Town Corridor</b>
Townhouse	SE	P
Multi Family House	SE	P
Commercial/Mixed-use - Small	P	P
Commercial/Mixed-use - Medium	P	NP

Single Story Commercial-Small	P	P
Single Story Commercial-Medium	P	NP

Key: P, Permitted, NP, Not Permitted, SE, Special Exception

A.

A. Building Lot Type Assembly

1. Additions, renovations and new buildings on existing built parcels, will require the assembly of at least one permitted building type by area as permitted in Table 16.91. Parcels that can meet the minimum building lot size requirements are permitted to assemble more than one building type.
2. Modifications to Building Lot Types. In the event an existing building exceeds the lot requirements as described below in B.1. Building Lot Type Parameter, the Town Planner or Appearance Review Board may recommend modifications to the building lot size if the existing or proposed building and/or additions do not exceed the overall block requirements as provided in Sec 1611 Site Design Standards A.3.b.

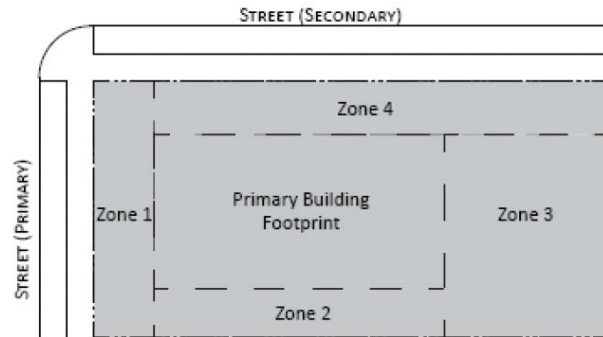
B. Building Type Parameters.

The following Building Lot Type parameters are included in the table provided for each building type. Each standard is labeled by a letter (A, example) which directly relates to the table provided on each building type. Building types shall be consistently applied by and not permitted a variance to increase in building size, however modifications of setbacks shall be permitted, when site constraints exist. The categories are described as follows:

1. Building Lot requirements, Provisions for minimum and maximums; lot depth, lot size and the permitted lot coverage and/or building footprint;
2. Building envelope: Provisions for the minimum and maximum setbacks permitted by front, side, and rear yards. The maximum and minimum frontage refers to the proportion of the lot width along which the primary building façade must be within the minimum and maximum front setback; and
3. Accessory Structure Envelope: provides the setbacks and maximum building footprints permitted for accessory structures.
4. Building height: Provisions for permitted heights in stories (st). The maximum height for first floor residential development shall be fourteen (14) feet and twenty (20) feet for non-residential. The maximum height for second story and higher shall not exceed twelve (12) feet. Each building lot typology provides the range of height appropriate for the building type. Height shall be measured from finished floor. Additional height may be permitted, through the Town's Special Exception Process.
5. Parking provisions: the Underlying Zoning Districts within the Gateway Corridor Overlay shall determine the amount of parking. Parking provisions provide zones where parking is permitted. The parking zone refers to any uncovered parking

1 area located on the parcel. Driveways are permitted in any zone provided the  
2 frontage requirements have been met as required by building type. The diagram  
3 illustrates a Primary and Secondary street. Primary streets are streets that are  
4 addressed to the parcel of land. Secondary streets may or may not have access  
5 to the parcel. Zones are defined and illustrated by the lot area between the  
6 principal building frontage and:

- 10 i. Zone 1: the right-of-way of
- 11 any primary street.
- 12 ii. Zone 2: any common
- 13 interior lot line.
- 14 iii. Zone 3: any rear lot line.
- 15 iv. Zone 4: the right-of-way of
- 16 any secondary street.



- 18 6. On corner lots, the primary
- 19 street shall be the street with the higher classification. If both streets are of equal
- 20 classification, the primary street shall be the street abutting the front lot line. On
- 21 through lots, the Town Planner shall determine which street or streets function as
- 22 primary streets and which function as secondary streets. Limited access roads
- 23 abutting lots shall not be considered primary streets for purposes of this section.

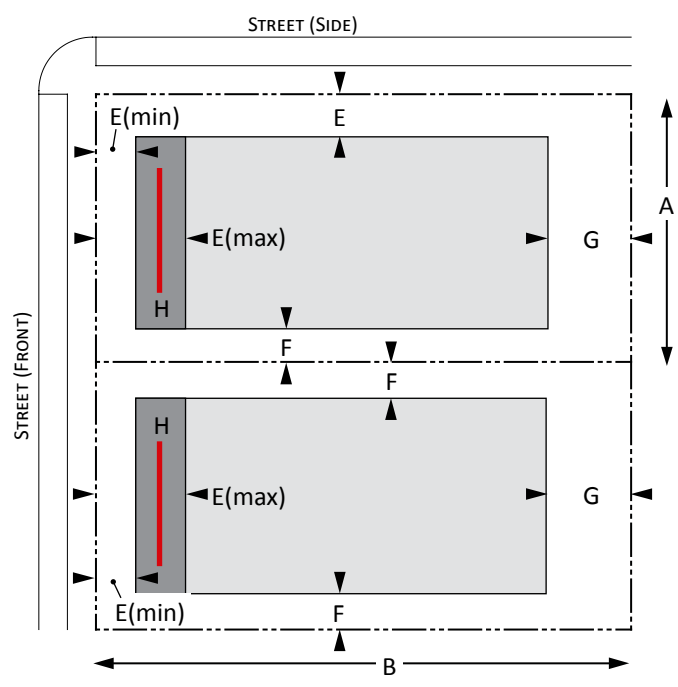


A building lot located and designed to accommodate multiple dwellings above or beside each other in a building that occupies most of its building lot width and is placed close to the sidewalk. Minimum units is 2 per building.

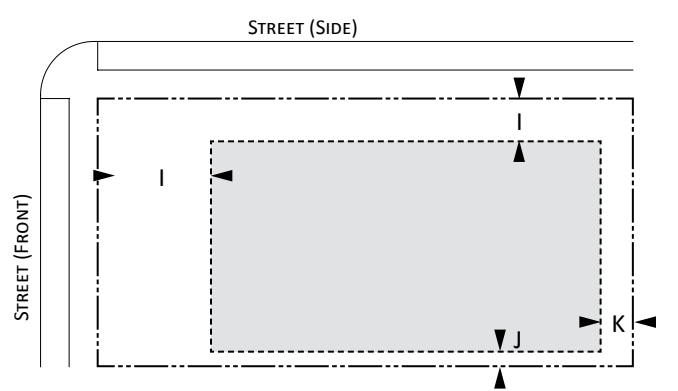
LOT REQUIREMENTS	MIN	MAX
A - Lot Width (ft)	40	120
B - Lot Depth (ft)	100	150
C - Lot Size (sf)	4,000	18,000
D - Lot Coverage (%)	--	50
BUILDING ENVELOPE	MIN	MAX
E - Street Setback (ft)		
Highway 50 Corridor (option 1)	10	20
Highway 50 Corridor (option 2)	35	40
Old Town Corridor	10	20
F - Side Setback (ft)(1)	10	--
G - Rear Setback (ft)	25	--
H - Frontage Buildout (%)	60	80
ACCESSORY STRUCTURE ENVELOPE	MIN	MAX
I - Street Setback (ft)	30 (1)	
J - Side Setback (ft)	10	--
K - Rear Setback (ft)	10	--
L - Building Footprint (sf)	--	800
BUILDING HEIGHT	MIN	MAX
M - Principal Building (st)	1	35'
N - Accessory Structure(s) (ft)	--	15
PARKING PROVISIONS		
Location	SSY, SY and RY	

(1) When adjacent to existing single family residential detached homes, the rear side setback shall be 25'.

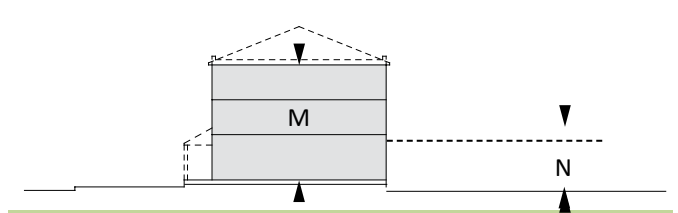
LOT REQUIREMENTS AND BUILDING ENVELOPE



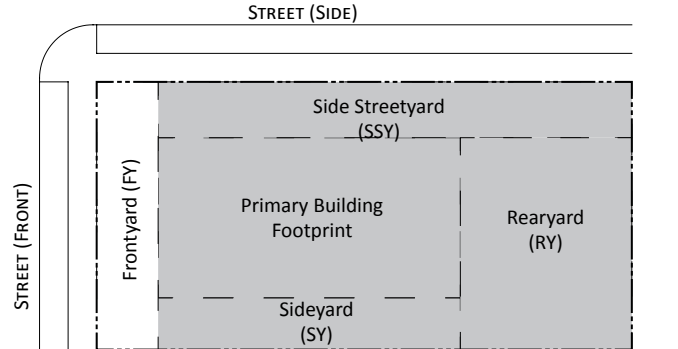
ACCESSORY STRUCTURE ENVELOPE



BUILDING HEIGHT



PARKING LOCATION

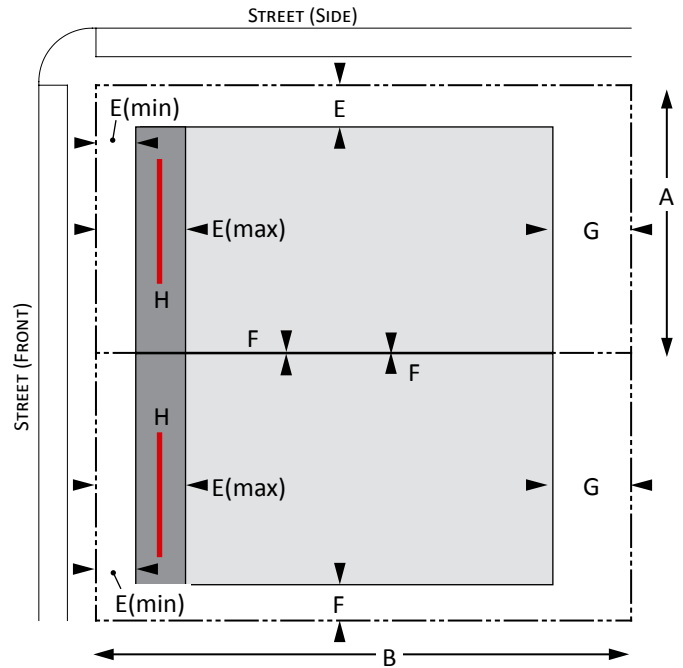


A building lot located and designed to accommodate a building with sidewalls on both side building lot lines and a private garden to the rear. This type may be used for residential uses and/or live work units as permitted by the underlying zoning district.

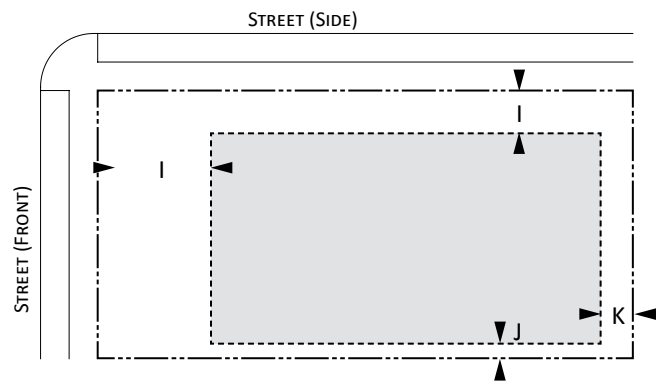
LOT REQUIREMENTS	MIN	MAX
A - Lot Width (ft)	15	25 <sup>^</sup>
B - Lot Depth (ft)	80	120
C - Lot Size (sf)	1,200	3,000
D - Lot Coverage (%)	--	60
BUILDING ENVELOPE	MIN	MAX
E - Street Setback (ft)	10	20
Highway 50 Corridor (option 1)	10	20
Highway 50 Corridor (option 2)	N/A	N/A
Old Town Corridor	10	20
F - Side Setback (ft)	0 *	--
G - Rear Setback (ft)	15	--
H - Frontage Buildout (%)	90	100
ACCESSORY STRUCTURE ENVELOPE	MIN	MAX
I - Street Setback (ft)	30	--
J - Side Setback (ft)	3	--
K - Rear Setback (ft)	10	--
L - Building Footprint (sf)	--	800
BUILDING HEIGHT	MIN	MAX
M - Principal Building (st)	1	24'
N - Accessory Structure(s) (ft)	--	15
PARKING PROVISIONS		
Location		RY

<sup>^</sup> End unit lot may be up to 10' larger.  
 \* 10' Separation required from adjacent detached building.  
 A maximum of 6 units may be provided in a row

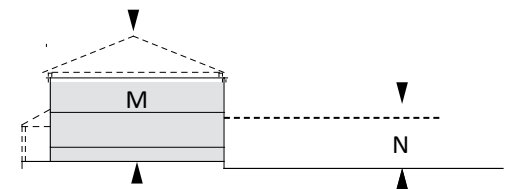
LOT REQUIREMENTS AND BUILDING ENVELOPE



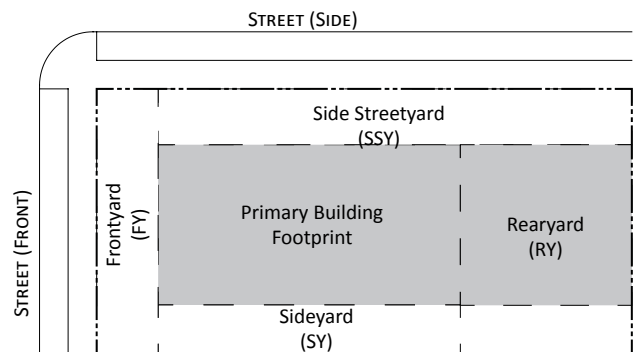
ACCESSORY STRUCTURE ENVELOPE



BUILDING HEIGHT



PARKING LOCATION

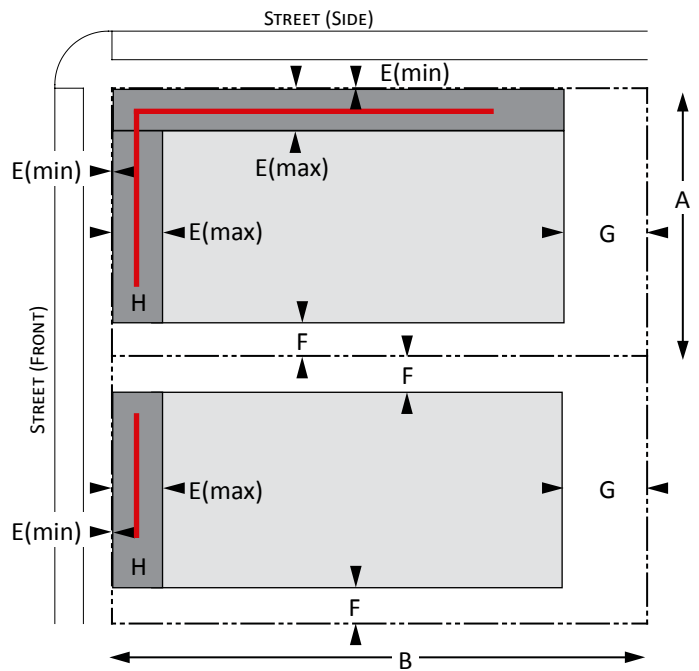


A building lot located and designed to accommodate a multi story building with commercial, office and/or multiple dwellings in any story that is designed for smaller lot sizes.

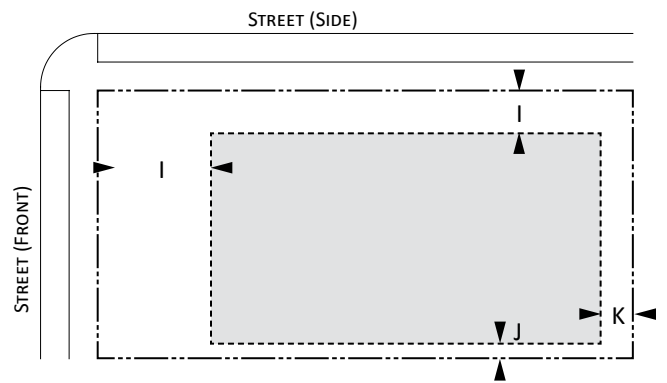
LOT REQUIREMENTS	MIN	MAX
A - Lot Width (ft)	15	60
B - Lot Depth (ft)	80	120
C - Lot Size (sf)	1,800	7,200
D - Lot Coverage (%)	--	90
BUILDING ENVELOPE	MIN	MAX
E - Street Setback (ft)(1)		
Highway 50 Corridor (option 1)	10	25
Highway 50 Corridor (option 2)	40	65
Old Town Corridor	10	20
F - Side Setback (ft)(2)	0	--
G - Rear Setback (ft)(2)	5	--
H - Frontage Buildout (%)	80	100
ACCESSORY STRUCTURE ENVELOPE	MIN	MAX
I - Street Setback (ft)	30	
J - Side Setback (ft)	3	--
K - Rear Setback (ft)	10	--
L - Building Footprint (sf)	--	800
BUILDING HEIGHT	MIN	MAX
M - Principal Building (st)	24'	35'
N - Accessory Structure(s) (ft)	--	15
PARKING PROVISIONS		
Location (3)	SY, SSY and RY	

- (1) If the Town require's a utility easement, the minimum setback may be 25 feet
- (2) When adjacent to existing single family residential detached homes, the rear and/or side setback shall be 25'.
- (3) If building is developed using Option 2 in Setbacks, parking shall be permitted in the Frontyard.

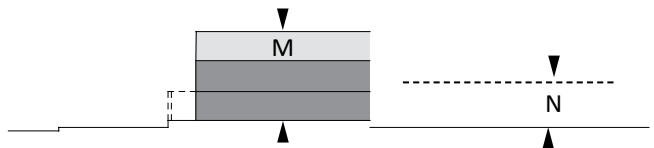
LOT REQUIREMENTS AND BUILDING ENVELOPE



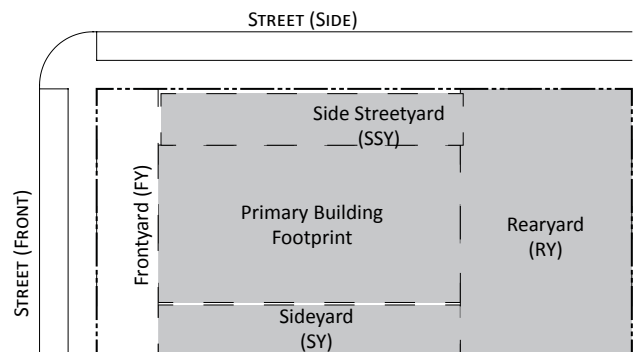
ACCESSORY STRUCTURE ENVELOPE



BUILDING HEIGHT



PARKING LOCATION





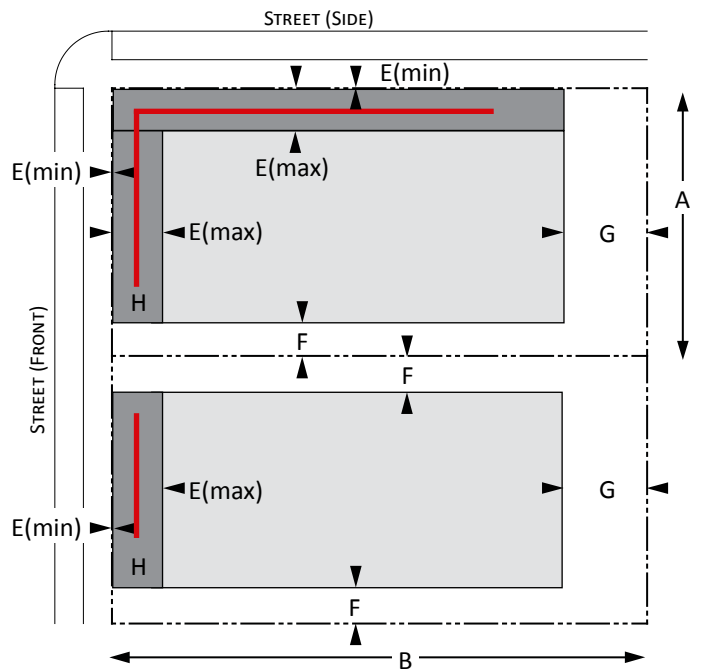
# COMMERCIAL/ MIXED-USE - MEDIUM

A building lot located and designed to accommodate a multi story building with commercial, office and/or multiple dwellings in any story that is designed for medium scale buildings.

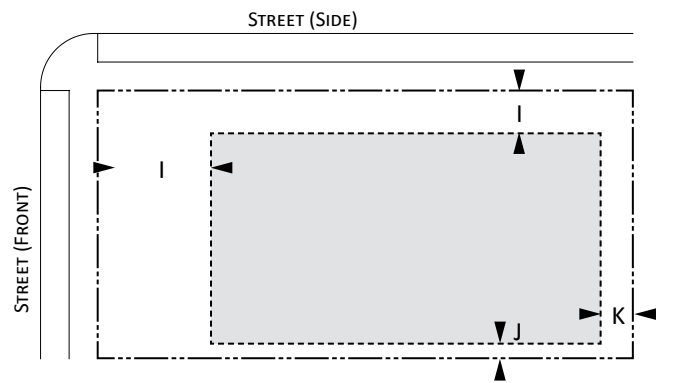
LOT REQUIREMENTS	MIN	MAX
A - Lot Width (ft)	60	120
B - Lot Depth (ft)	--	500
C - Lot Size (sf)	--	60,000
D - Lot Coverage (%)	--	90
BUILDING ENVELOPE	MIN	MAX
E - Street Setback (ft)(1)		
Highway 50 Corridor (option 1)	10	25
Highway 50 Corridor (option 2)	40	65
Old Town Corridor	10	20
F - Side Setback (ft)(2)	0	--
G - Rear Setback (ft)(2)	10	--
H - Frontage Buildout (%)	90	100
ACCESSORY STRUCTURE ENVELOPE	MIN	MAX
I - Street Setback (ft)	30	
J - Side Setback (ft)	10	--
K - Rear Setback (ft)	10	--
L - Building Footprint (sf)	--	800
BUILDING HEIGHT	MIN	MAX
M - Principal Building (st)	24'	35'
N - Accessory Structure(s) (ft)	--	15
PARKING PROVISIONS		
Location (3)	SY and RY	

- (1) If the Town require's a utility easement, the minimum setback may be 25feet
- (2) When adjacent to existing single family residential detached homes, the rear and/or side setback shall be 25'.
- (3) If building is developed using Option 2 in Setbacks, parking shall be permitted in the Frontyard.

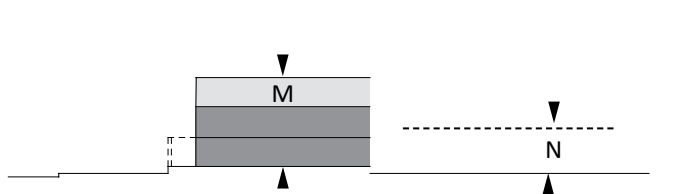
## LOT REQUIREMENTS AND BUILDING ENVELOPE



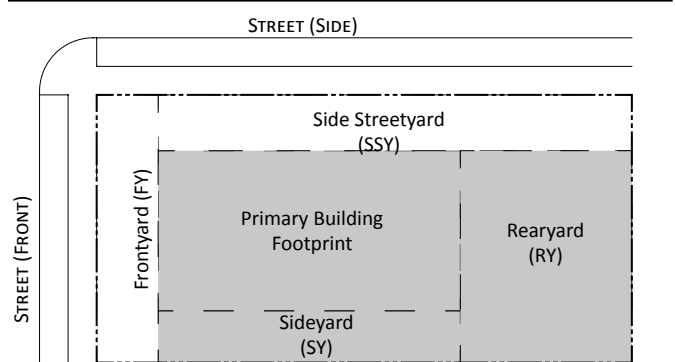
## ACCESSORY STRUCTURE ENVELOPE



## BUILDING HEIGHT



## PARKING LOCATION



# SS1

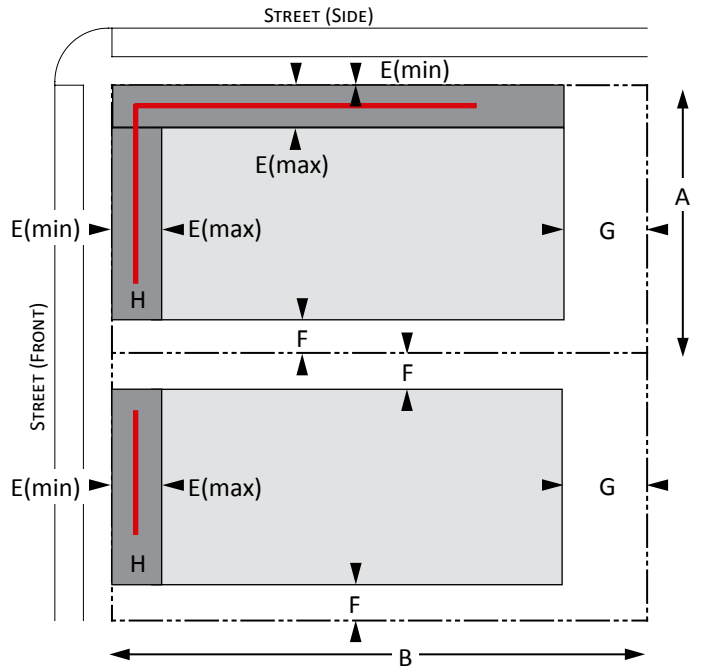
## SINGLE STORY COMMERCIAL BUILDING-SMALL

A building lot located and designed to accommodate single use office and retail.

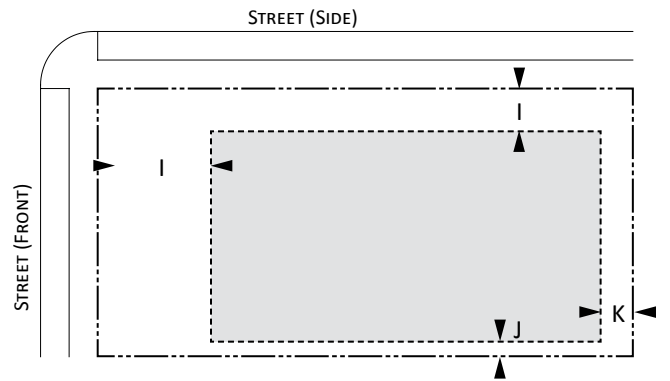
LOT REQUIREMENTS	MIN	MAX
A - Lot Width (ft)	30	60
B - Lot Depth (ft)	120	120
C - Lot Size (sf)	3,600	7,200
D - Lot Coverage (%)	--	60
BUILDING ENVELOPE	MIN	MAX
E - Street Setback (ft) (1)		
Highway 50 Corridor (option 1)	10	25
Highway 50 Corridor (option 2)	40	65
Old Town Corridor	10	20
F - Side Setback (ft)(2)	0	--
G - Rear Setback (ft)(2)	15	--
H - Frontage Buildout (%)	50	100
ACCESSORY STRUCTURE ENVELOPE	MIN	MAX
I - Street Setback (ft)	30	
J - Side Setback (ft)	10	--
K - Rear Setback (ft)	10	--
L - Building Footprint (sf)	--	800
BUILDING HEIGHT	MIN	MAX
M - Principal Building (st)	1	1
N - Accessory Structure(s) (ft)	--	15
PARKING PROVISIONS		
Location (3)	SY, SSY and RY	

- (1) If the Town require's a utility easement, the minimum setback may be 25feet
- (2) When adjacent to existing single family residential detached homes, the rear and/or side setback shall be 25'.
- (3) If building is developed using Option 2 in Setbacks, parking shall be permitted in the Frontyard.

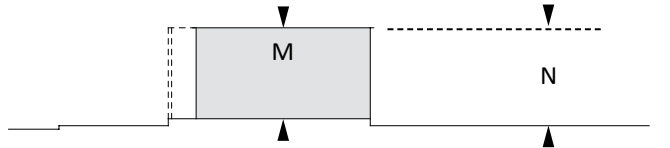
### LOT REQUIREMENTS AND BUILDING ENVELOPE



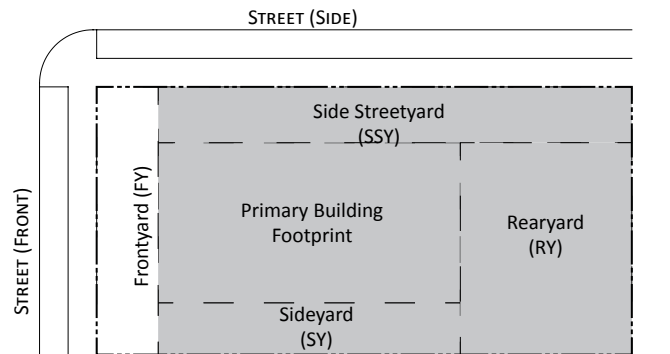
### ACCESSORY STRUCTURE ENVELOPE



### BUILDING HEIGHT



### PARKING LOCATION



# SS-2

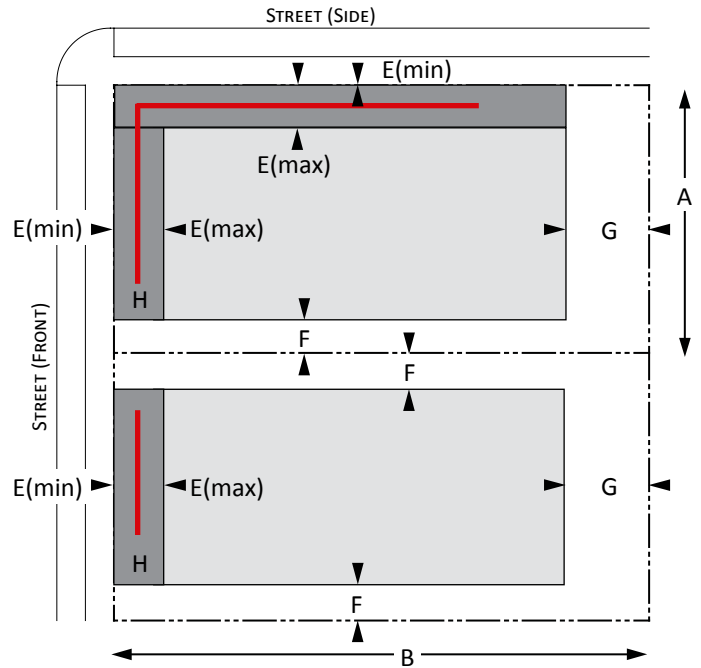
## SINGLE STORY COMMERCIAL BUILDING-MEDIUM

A building lot located and designed to accommodate single use office and retail.

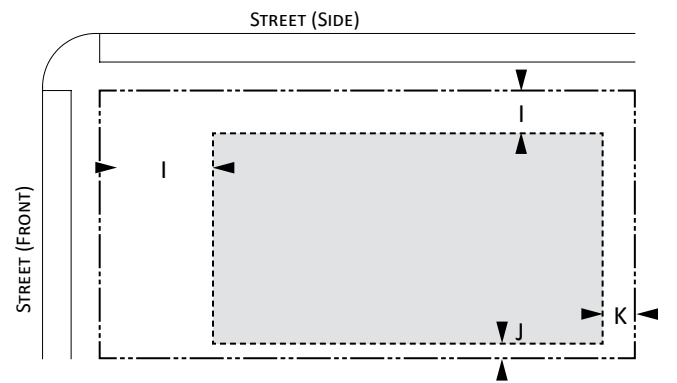
LOT REQUIREMENTS	MIN	MAX
A - Lot Width (ft)	50	200
B - Lot Depth (ft)	100	--
C - Lot Size (sf)	7,500	60,000
D - Lot Coverage (%)	--	60
BUILDING ENVELOPE	MIN	MAX
E - Street Setback (ft)(1)		
Highway 50 Corridor (option 1)	10	25
Highway 50 Corridor (option 2)	40	65
Old Town Corridor	10	20
F - Side Setback (ft)(2)	0	--
G - Rear Setback (ft)(2)	15	--
H - Frontage Buildout (%)	50	100
ACCESSORY STRUCTURE ENVELOPE	MIN	MAX
I - Street Setback (ft)	30	
J - Side Setback (ft)	10	--
K - Rear Setback (ft)	10	--
L - Building Footprint (sf)	--	800
BUILDING HEIGHT	MIN	MAX
M - Principal Building (st)	1	1
N - Accessory Structure(s) (ft)	--	15
PARKING PROVISIONS		
Location (3)	SY, SSY and RY	

- (1) If the Town require's a utility easement, the minimum setback may be 25feet
- (2) When adjacent to existing single family residential detached homes, the rear and/or side setback shall be 25'.
- (3) If building is developed using Option 2 in Setbacks, parking shall be permitted in the Frontyard.

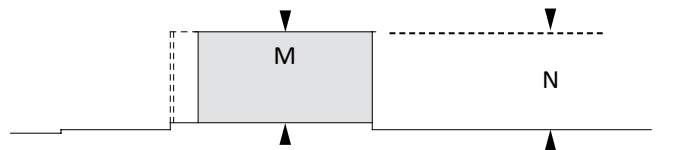
### LOT REQUIREMENTS AND BUILDING ENVELOPE



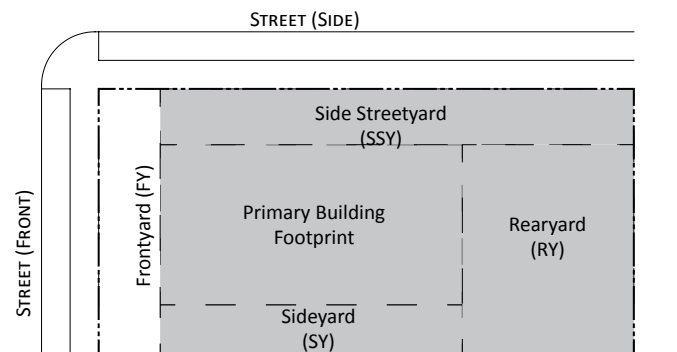
### ACCESSORY STRUCTURE ENVELOPE



### BUILDING HEIGHT



### PARKING LOCATION



1  
2  
3  
4  
5  
6  
7  
8

**Section 16.10 Required Landscaping and Buffering**

All site plans submitted must include a detailed Landscape Plan delineating the location, height, and type of all plant and groundcover materials, as well as the irrigation system. Likewise, area calculations must be given for all landscaping. This plan must be prepared by a Landscape Architect licensed to practice in the State of Florida. Additionally, the following standards shall to apply to the specified corridors:

Table 16.10.1 Landscape Specifications		GATEWAY CORRIDORS	
		Highway 50 Activity Corridor	Old Town Corridor
<b>Shade Tree Specifications,</b> spacing and number of trees is by buffer type		3" caliper, 14' height, 4' clear trunk. Shade tree must also be a year round shade tree species.	
<b>Understory Tree Specifications,</b> spacing and number of trees is by buffer type		8' height, 2" caliper	
<b>Shrubs, Ground Cover Specifications</b> overall standards in buffers, vehicular use areas, landscape islands.		50% of shrubs, ornamental grasses and groundcovers to provide 100% coverage of the landscape area within 1 growing season no more than 50% of the groundcover can be mulch, sod or landscape material. Option 1, in Highway 50 Activity Corridor does not require shrubs or groundcovers.	

9

**A. Native Landscape**

Drought tolerant landscaping that promotes good horticulture practice for efficient use of water shall be used for all new development within the Gateway Corridor Overlay. Installed canopy/understory trees and plant materials shall be grouped together with plants of the same water use needs into zones.

10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26

1. A minimum of seventy five percent (75%) of all trees and fifty percent (50%) of all shrub and groundcover plant material shall be drought-tolerant native or naturalized species that do not require supplemental irrigation beyond typically naturally occurring precipitation. Drought-tolerant species shall be listed in the Florida Friendly Landscaping and Waterwise Florida Landscapes guides published by the South Florida Water Management District or any recognized and approved list of drought-tolerant species as accepted by the County.
2. Sod areas shall be limited to those areas that receive pedestrian traffic, recreation uses, or erosion control.
3. Mulch shall be installed in all plant beds and around trees in sod areas, and shall be supplemented as required to maintain 3" mulch depth, except

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29

for groundcover plant materials.

B. Irrigation

When irrigation systems are installed, the use of low volume, emitter, or target irrigation shall be used for trees, shrubs and groundcovers. A moisture sensor and/or rain gauges shall be provided with automatic irrigation system.

1. Existing landscape irrigation systems located within the Gateway Corridors shall accept low quality water services when available as follows:

- a. For all new irrigation installations, the property shall connect to re-use water service if available.
- b. If the existing irrigation system is using city potable water for irrigation, the property shall connect, when available, to re-use water service.
- c. If the existing irrigation system is using lake water or stormwater for irrigation, then such property shall continue to use that low quality water.

C. Landscape Standards. The following table provides all the landscape requirements for a parcel.

Table 16.10.2 Landscape Standards				
		Highway 50 Activity Corridor		Old Town Corridor
Shade Trees	Public Street Buffer	Option 1, when a building is setback a min 10'-20' from the ROW line	Option 2, when a building is setback min 40' from the ROW line	All areas within Old Town Corridor
	Buffer Width	10' minimum buffer (includes sidewalk, seating area, hardscape elements)	25' minimum buffer (may include sidewalk, seating area, hardscape) for SR 50, other roadways 15'.	4' min (excluding sidewalks) and 75% of the frontage



	Utility Easement	Up to 10' of an area that may be within the Buffer Width dimension	Up to 10' of an area that may be within the Buffer Width dimension	N/A
	Spacing	1 tree for every 30' on center of fraction thereof	1 tree for every 40' On Center or fraction thereof). The required trees may be clustered, but spacing may not exceed 50'	
	Planting Standards	Sidewalk planter- min 5 x5 foot planter. Canopy tree in tree grate may be installed in tree pits with a pedestrian grate and be ADAAG compliant	Open landscape areas or landscape islands with an area of four hundred (400) square footage. or more do not require soil volume extension measures.	
	Between Parcels	No understory trees are required	2 per 100 linear feet.	No requirement
	Building Perimeter	1 shade or understory per linear feet of building perimeter		
	Parking Lots	A minimum of (1) tree per (4) parking spaces. At least 50% of these trees must be shade trees. Rows of parking shall be broken every 10 spaces by a landscape break a minimum of 80 square feet in size. Rows of parking shall not be longer than 90 feet (10 spaces) without a landscape island. The landscaped parking island shall have a minimum planting area of 360 square feet if back to back, and 180 square feet if on a single row		
	Pedestrian Walkways	(1) understory or shade tree per 30 linear feet of walkway		
Understory Trees	Front Buffer	Option 1, when a building is setback a min 10'-20' from the ROW line	Option 2, when a building is setback min 40' from the ROW line	All areas within Old Town Corridor
	Spacing	No understory trees are required	4 per linear feet	
	Between Parcels	No understory trees are required	2 per 100 linear feet.	No requirement
	Building Perimeter	1 shade or understory per linear feet of building perimeter		
	Parking Lots	a minimum of (1) tree per (4) parking spaces. At least 50% of these trees must be shade trees. Rows of parking shall be broken every 10 spaces by a landscape break a minimum of 80 square feet in size.		

		Rows of parking shall not be longer than 90 feet (10 spaces) without a landscape island. The landscaped parking island shall have a minimum planting area of 360 square feet if back to back, and 180 square feet if on a single row.	
	<b>Pedestrian Walkways</b>	(1) understory or shade tree per 30 linear feet of walkway	
<b>Shrubs and Groundcover</b>	<b>Front Buffer</b>	Not required	As required/defined at the top of the table under specifications
	<b>Between Parcels</b>	As required/defined at the top of the table under specifications	
	<b>Rear Buffer</b>	When adjacent/abutting residential six-foot high brick screening walls shall be required the length of the property. Where visible from any right-of-way, these walls will also have a three-foot high (within one year of planting) hedge on the side facing the right-of-way and running the length of the wall where visible.	
	<b>Open Areas</b>	Shall be landscaped completely with shrubs, groundcovers, annuals or sod	

1  
2 D. *Water Retention/Detention Areas*: The shape of a manmade body of water, including  
3 wet retention areas, shall be designed to appear natural by having off-sets in the edge  
4 alignment that are a minimum of ten feet and spaced 50 feet apart.

5 1. Natural and manmade bodies of water, which are located adjacent to a public  
6 right-of-way, and are greater than 20,000 square feet in area, shall be  
7 incorporated into the overall design of the project in at least one of the following  
8 ways:

- 9 a. Provide a minimum eight-foot wide walkway with trees an average  
10 of 50 feet on center and shaded benches a minimum of six-feet long  
11 (or picnic tables), every 150 linear feet;
- 12 b. Provide a public access pier with covered structure and seating,  
13 and appropriate pedestrian access; or
- 14 c. Provide a plaza/courtyard, 200 square feet minimum, with shaded  
15 benches and/or picnic tables adjacent to the water body.

16 2. All stormwater facilities will be designed so as to not require fencing. Fenced  
17 stormwater facilities shall only be approved in extreme situations, and will be  
18 located to the side and/or rear of the property so as to be as far from the adjacent  
19 right-of-way as possible. Fences shall be non-chain link, black or forest green,  
20 decorative yet functional, and in good condition. The design of the storm water  
21 facilities must be consistent with St. Johns River Water Management Design  
22 Specifications.

23

1 Section 16.11 Site Design Standards

2 A. Circulation Standards

- 3 1. Vehicular Access: All individual developments will integrate shared access and  
4 access points among adjoining parcels. Cross-access between adjacent parcels  
5 shall be required to minimize congestion on public streets and decrease the  
6 number of potential traffic conflicts. To preclude many of the traffic hazards of  
7 poor access management, the use of "rearage" roads will be required where  
8 deemed necessary by the Town Engineer. To protect the health, safety, and  
9 welfare of motorists traveling this roadway, not all parcels will be granted a curb-  
10 cut. It will be determined by the Town Transportation Engineer if said curb-cuts  
11 are absolutely necessary, or if access can be obtained from an alternative  
12 location. The Town retains the right to limit access to state and county rights of  
13 way, regardless of any access otherwise granted by the State of Florida or Orange  
14 County. Master-planned access for properties fronting on Highway 50 may be  
15 required. A vehicle stacking area as determined by the Town Transportation  
16 Engineer may be required on principal driveways leading out of the project, based  
17 on the principal use and traffic characteristics.
- 18
- 19 2. Circulation plan: For proposed development that is less than three acres in size, a  
20 circulation plan shall be submitted which identifies potential cross-and joint-access  
21 to adjacent parcels. Pedestrian accommodations and alternative transportation  
22 modes shall be reflected in the submitted circulation plan.
- 23
- 24 3. Block plan: When a proposed development is three acres or greater, a block plan  
25 shall be required as part of the proposed development which requires the  
26 proposed development to construct a block assembly as part of the development.  
27 An applicant must submit the block length, block perimeter and the street type that  
28 will be constructed for the development. Streets shall be designed to allow for  
29 adjacent parcels to connect to existing streets as follows:
- 30 a. *Circulation standards for new streets.*
- 31 i. Streets are intended for use by vehicular and pedestrian traffic  
32 and to provide access to lots and open spaces.
- 33 ii. Circulation standards shall generally consist of vehicular lanes,  
34 parking, bicycle lanes, landscape, sidewalks, and outdoor seating.
- 35 iii. Pedestrian comfort shall be a primary consideration of the street.  
36 Design conflict between vehicular and pedestrian movement  
37 generally shall be decided in favor of the pedestrian.
- 38 b. *Block Perimeter and Block Size.* The street network shall be designed  
39 around a series of blocks. In order to create a system of land subdivision  
40 and development which links one area to another, land should be  
41 organized by development blocks to the maximum extent feasible given  
42 the topography and physical characteristics of each individual site. A block  
43 perimeter shall be measured as the sum of lot frontages. Block perimeter  
44 shall not exceed a total length of 1600 feet and the maximum block length  
45 shall generally be 400 feet.
- 46 c. All Streets shall terminate at other streets, in order to form a network.

1 Internal streets shall connect wherever possible to those on adjacent  
2 sites. Cul-de-sacs shall be subject to approval to accommodate specific  
3 site conditions only.

4 d. Each lot shall front a street, pedestrian passage, or civic space.  
5

6 4. Pedestrian Walkways. Pedestrian ways, linkages, and paths shall be provided  
7 from the building entry(s) to surrounding streets, external sidewalks, and  
8 outparcels. Pedestrian ways shall be designed to provide access between parking  
9 areas and the building entrance in a safe and coordinated manner. Pedestrian  
10 ways may be incorporated within a required landscape buffer, provided said buffer  
11 is not less than 10 feet wide on average. Shared pedestrian walkways are  
12 encouraged between commercial parcels with the following design requirements:  
13

14 a. Pedestrian ways located adjacent to rights-of-way will be serpentine in and will  
15 be a minimum of eight feet wide. All other pedestrian ways will be a minimum  
16 of six feet wide.

17 b. Materials used will be concrete, colored concrete, specialty pavers, or  
18 stamped pattern concrete. Where pedestrian crosswalks are needed at the  
19 building perimeter, said walks will be identified with signage and/or variations  
20 in pavement materials or markings.

21 c. Brick or other specialty pavers meeting the approval of the Appearance  
22 Review Board, shall be provided at all right-of-way crosswalks and be a  
23 minimum of eight feet from the intersection of the pedestrian path and the  
24 right-of-way.

25 d. Each parcel shall provide a seating area for pedestrians along the portion of  
26 the pedestrian path which fronts their tract. A bench and trash receptacle  
27 shall be located on said area.

28 e. Site Furnishings: A minimum of (2) benches and (1) trash receptacle will be  
29 provided per parcel, or per 400 linear feet of frontage, which ever is greater.  
30 For parcels with frontage on two public roads, this section shall apply to both  
31 frontages. Where benches are located adjacent to a right-of-way, all benches  
32 will be positioned so as to face that right-of-way. Additionally, each developed  
33 parcel shall provide (1) bicycle rack per 400 feet of frontage, or fraction  
34 thereof. All benches, bike racks, and trash receptacles will be antique in  
35 design and have all metal surfaces painted black or forest green.  
36

37  
38 B. **Building Frontage.** Building frontages shall occupy no less than 75% of SR 50 and  
39 Oakland Avenue street and 50% of any other street facing entrance. Gasoline canopies,  
40 drive thru aisle lanes may not be located in the building frontage area. They shall be  
41 located to the side or rear of the primary building. If site constraints exist, a knee wall may  
42 be constructed with the following provisions.

43  
44 1. Only 25% of the required frontage may be credited as part of a knee wall.

- 1                   2. A knee wall must be constructed with a minimum of 2 feet in height to a maximum
- 2                   of 3 feet in height
- 3                   3. Opaque material which complements the primary building's architecture style.
- 4                   4. The knee wall should be the length of the primary building frontage.
- 5
- 6           C. Public Entrance. Buildings that are open to the public shall have an entrance for
- 7           pedestrians from the street to the building interior. This entrance shall be designed to be
- 8           attractive and functionally be a distinctive and prominent element of the architectural
- 9           design, and shall be open to the public during business hours. Buildings shall incorporate
- 10           lighting and changes in mass, surface or finish to give emphasis to the entrances.
- 11
- 12           D. Building Façade. Buildings shall provide a foundation or base, typically from ground to
- 13           bottom of the lower windowsills, with changes in volume or material. A clear visual division
- 14           shall be maintained between the ground level floor and upper floors with either a cornice
- 15           line or awning from 12' to 16' above grade, whichever applies to the proposed
- 16           development. No more than 20 feet of horizontal distance of wall shall be provided without
- 17           architectural relief for building walls and frontage walls facing the street. All buildings
- 18           excluding single family detached homes shall utilize at least three of the following design
- 19           features to provide visual relief along all elevations of the building:
  - 20                   1. Divisions or change in materials (materials should be drawn from a common
  - 21                   palette)
  - 22                   2. Window bays
  - 23                   3. Separate entrances and entry treatments, porticoes extending at least five (5) feet.
  - 24                   4. Variation in roof lines
  - 25                   5. Dormers
  - 26                   6. Canopies, extending at least five (5) feet
  - 27                   7. Gables
  - 28                   8. Overhang extending at least five (5) feet
  - 29                   9. Recessed entries (at least three [3] feet from the primary façade)
  - 30                   10. Protruding entries (at least three [3] from the primary façade)
  - 31                   11. Covered porch entries
  - 32                   12. Cupolas
- 33
- 34           E. Storefront character. Buildings shall express a "storefront character". This guideline is met
- 35           by providing all of the following features along the building frontage as applicable.
  - 36                   1. Corner building entrances on corner lots.
  - 37                   2. Regularly spaced and similar-shaped windows with window hoods or trim (all
  - 38                   building stories).
  - 39           Large display windows on the ground floor. All street-facing, park-facing and plaza-facing
  - 40           structures shall have windows covering a minimum of 40% and a maximum 80% of the
  - 41           ground floor of each storefront's linear frontage. Blank walls shall not occupy over 50% of
  - 42           a street-facing frontage and shall not exceed 20 linear feet without being interrupted by a

1 window or entry. Mirrored glass, obscured glass and glass block cannot be used in  
2 meeting this requirement.

3 F. Orientation. The primary building entrances shall be visible and directly accessible from a  
4 public street. Building massing such as tower elements shall be used to call-out the  
5 location of building entries.

6

7 G. Parking. Parking areas and driveways shall be designed to establish a logical pattern of  
8 pedestrian access, traffic flow, and parking lots with visible connections between building  
9 entrances, parking lot entrances, roads, parking spaces, sidewalks, and adjacent projects.

10

11

12 H. Basic Design Parameters. The following standards are basic minimum guidelines for any  
13 development within the Gateway Corridor. Applicants will appear before the Town's  
14 Review Board, and final determination of design details, modifications shall be made as  
15 part of the Appearance Review Board's Actions and recommendations.

16

17 1. Façade: Exterior-building materials shall consist of, or accurately resemble brick,  
18 or horizontal or vertical wood siding. Historically correct architectural details, such  
19 as door framing, window framing, and corner framing shall be provided. Exposed  
20 concrete block shall not be allowed. The side of the building that is facing or  
21 backing up to any residential development must be treated with the same  
22 architectural design standards as the front of the building. Alternative exterior  
23 building materials appropriate within the Corridor area shall include coquina  
24 stone/facing, or shell-based stucco, provided that such materials comprise no  
25 more than one-third of any building elevation visible from a public right-of-way.

26 2. Roofs: All roof materials should be comprised of metal shingles, corrugated metal  
27 sheet, V-crimp metal sheet, or standing seam metal sheet. Alternative roof  
28 materials may be considered, so long as they meet the historical design intent of  
29 this Ordinance. Metal roofs, if painted, shall be painted a color compatible with the  
30 color scheme of the structure

31 3. All buildings shall incorporate sloped roofs. All roofs will be required to display  
32 exposed functional or non-functional rafters with an overhang. Flat roofs may be  
33 permitted for up to one-fourth of the building footprint, provided that a sloped roof  
34 structure with exposed rafters and a decorative tower, cupola, or railing is  
35 provided. Flat roofs must also have a cornice detail consisting of, or accurately  
36 simulating, wood, and projecting from the building face. Where flat roofs are  
37 integrated into predominately sloping roof structures, the top shall be finished with  
38 a decorative railing. Where hip roofs are utilized, a cupola shall be provided. The  
39 minimum permitted slope shall be 8:12. The use of dormers is strongly  
40 encouraged. Entirely flat roofs may be considered if the structure obviously meets  
41 the historical design intent of this ordinance. In such cases, careful consideration  
42 will be given to how well the applicant has integrated other elements of this  
43 ordinance within the project.

44 4. Windows: False or real windows shall be provided on all elevations visible to

1 motorists or pedestrians. Window openings shall appear to be vertical in nature  
2 with a minimum of one and one-half (1.5) feet of height for every one foot of width.  
3 Secondary windows situated on the sides or rear of the building, in a clerestory  
4 with lower windows, in the gables, or in dormers, may have a proportion of one (1)  
5 foot of height for every one (1) foot of width provided such windows are divided  
6 with fixed mullions, on the interior. False mullions on the interior of glass are  
7 prohibited. Windows shall appear to be casement or double-hung. The use of  
8 fixed glass and/or false exterior mullions shall be permitted. The use of closed  
9 shutters, three-sided fabric awnings, spandrel glass, or other appropriate  
10 vernacular architectural feature shall be permitted to achieve the vertical look.

- 11 5. Shutters. For windows with shutters, the shutter dimensions shall be appropriately  
12 scaled to the window so as to give the appearance of operable shutters. In the  
13 event that shutters are used to create verticality as required by (6) above, the  
14 shutters do not have to provide the appropriate vertical dimension to fully cover the  
15 window.
- 16 6. Colors: Exterior building materials, unless natural stone or brick, shall be painted  
17 with earth-tone colors. Where two or more exterior building materials are utilized,  
18 each shall be painted a different color. All wood trim, rafters, eaves, corner trim,  
19 window trim, brackets, fences, and other wood trim or supporting components  
20 shall be painted a contrasting light color. Doors, garage doors, windows, and  
21 shutters shall be painted a non-white color that is different from the exterior  
22 building materials. No two buildings that share the same property line shall be  
23 permitted to have exterior building materials painted the same color. Dayglow,  
24 fluorescent, and visually overwhelming colors which are in stark contrast to  
25 surrounding buildings, and which call undo attention to the property, shall not be  
26 permitted. Likewise, color schemes that are determined by the Appearance  
27 Review Board to be excessive, gaudy, or otherwise out of character with the spirit  
28 of this Ordinance, shall also be prohibited.
- 29 7. Corporate Logo Colors. Corporations commonly use colors as an identifying  
30 building feature and will be regulated as part of Signage in Sec 16.12.. Where  
31 color schemes are used that commonly identify the business on site, said areas  
32 shall be considered signage, and shall be included in the calculation of sign area.
- 33 8. Architectural Design Alternative: Applicants have the option of submitting plans  
34 which may differ somewhat from the design specifics of the above criteria (basic  
35 design parameters), if:
  - 36 a. Said plans offer both innovative design, and sufficient additional  
37 amenities in lieu of any items listed above that will not be included on  
38 site; and
  - 39 b. Said plans do not attempt to alter site design standards outside those  
40 listed in this section;
  - 41 c. Said plans, when submitted, specifically state that the plans are  
42 submitted using the "Architectural Design Alternative", and all areas  
43 where the proposed design differs from this Ordinance are well  
44 delineated.

- 1 d. Said plans shall be reviewed by the Town's Appearance Review  
2 Board.
- 3 4. *Blank Wall Areas*: Elements of design such as color, shape, architectural  
4 banding, pattern change, material and form change, etc, shall be used to  
5 break up large areas of blank wall space that are visible from any right-of-  
6 way or parking area. Blank areas shall not exceed 20 feet in horizontal  
7 direction, or 10 feet in vertical direction.

## 8 Section 16.12 Signs; Nonconforming Signs

9 The movement today for most cities is to reduce the number, size, and height of signs in an effort  
10 to eliminate visual clutter and maintain roadway aesthetics. An excessive display of signage can  
11 be a nuisance, a traffic hazard, and confusing. Excessive signage is unaesthetic and detracts from  
12 development without regard for landscaping or architecture. The following sign standards shall be  
13 required to have a sign permit and the requirements for signage are as follows.

### 14 A. Signage for individual commercial or industrial projects:

- 15 1. Wall signs. The maximum allowable wall sign area shall be 1.5 square feet per one linear  
16 foot of building frontage. Total sign area shall be the sum of all sign areas excluding  
17 window signs or opening banners. No individual wall sign shall exceed 80 square feet in  
18 size. Maximum vertical dimension shall not exceed 25 percent of the building height.  
19 Additional wall sign area of one square foot per tenant front foot shall be allowed for  
20 miscellaneous wall signage to include window signs, poster frames, and similar signs  
21 supplementary to identification signs. These signs must be located inside the building.  
22 Additionally:
- 23 2. Ground signs:
- 24 a. Only one ground sign shall be allowed per developed parcel. All ground signs shall  
25 include street number(s) of location.
- 26 b. All signs will be wide-based monument style. Pylon signs may be permitted when  
27 the pylon is effectively screened from view with landscaping or berm features
- 28 c. Any external above ground light source shall be located and hidden within a planter  
29 bed. Light sources located outside planter beds shall be burial fixtures.
- 30 d. The maximum height of the entire sign shall be 8 feet above the elevation of the  
31 nearest sidewalk.
- 32 e. The setback for all monument signs shall be a minimum of five feet from the right-of-  
33 way. Spacing shall be one hundred (100) feet or greater for all signs.
- 34 f. The maximum allowable ground sign area for individual structures shall be 48 square  
35 feet along Highway 50, and 32 square feet along all other roadways.
- 36 3. Window Signs: In no case shall more than 30% of the window area of a building be  
37 covered with window signage, stickers, banners marquis, or similar advertising devices.

38  
39



1 B. Shopping centers: Shopping centers may be permitted signage under this section. However,  
2 the following criteria are to be considered guidelines for maximum signage. All shopping  
3 center signage shall be reviewed by the Town as to final size, location and coordination,  
4 colors, design, and materials as a part of the site plan approval process.

5 1. Monument Signs:

- 6 a. Centers under 75,000 square feet: Shopping center identification signage of 32  
7 square feet or less. Two major tenant signs of 12 square feet or less, each.
- 8 b. Centers of 75,000 square feet or larger: Shopping center identification sign of 48  
9 square feet or less. Three major tenant signs of 12 square feet or less, each.
- 10 c. Maximum height shall be 12 feet, setbacks from side lot lines shall be 50 feet, or  
11 equidistant.

12 2. Wall signs for individual tenants within centers:

- 13 a. Maximum sign area shall be determined by multiplying 80 percent of the tenant front  
14 feet by two (2) feet. In the case of corner stores, additional signage may be allowed  
15 only where the same or similar façade treatment is used on both front and side. Sign  
16 area is not transferable between facades.
- 17 b. Maximum sign height for business having 50 front feet or less shall be two feet. For  
18 anchor tenants with specialized architectural treatments that identify them as such,  
19 maximum height shall not exceed 25 percent of the building height.

20 3. Window Signs: In no case shall more than 30% of the window area of a building be  
21 covered with window signage, stickers, banners marquis, or similar advertising devices.

22 4. Maximum total sign area. In no case shall the sum of total wall sign square footage  
23 exceed two square feet per one linear foot of building frontage on any one parcel.

24 5. Message. Neither the sign nor its pans shall move, rotate or use flashing lights.

25 6. Illumination. Sign lights shall be focused direct, and so arranged as to prevent glare or  
26 direct illumination or traffic hazard from said lights onto the abutting roadways or  
27 residential districts. No flashing or pulsating lights shall be permitted on any sign.

28 7. Prohibited signs. Off premise signs, portable signs, pole signs not constructed similar to  
29 Illustration 13, temporary signs, bench signs, or any signs which are affixed to an object  
30 for the purpose of advertising goods or services whose location and description are not  
31 well delineated in the most current approved site plan for that parcel.

32 8. Real estate signs. All real estate signs as defined and controlled by code requirements.

33 9. Flags. Flags are permitted as follows: a maximum of one state, one federal and one local  
34 county flag per parcel each a maximum of 35 square feet.

35 10. Non-conforming signs: Any sign having original cost in excess of \$100 and which is non-  
36 conforming as to permitted sign area or any other reason may be maintained a period of  
37 from three to five years from the installation date, or the most recent renovation date. The  
38 term of years to be determined by the cost of the sign or of renovation, including cost,  
39 shall be as follows:

40 Sign Cost or Renovation Cost

1	\$ 101.00 to \$1,000.00	3 years
2	\$1,001 to \$3,000.00	4 years
3	\$3,001.00 and above	5 years

4 a. Any owner of a sign who desires to rely upon an amortization period of longer than  
5 three years shall file with the Planning and Zoning Board, within one year from the  
6 effective date of this Ordinance, a statement setting forth the cost and the date of  
7 most recent renovation, and a written agreement to remove the non-conforming sign  
8 at or prior to the expiration of the amortization period for that sign.

9 b. Any non-conforming sign with a value of less than \$100 shall be removed  
10 immediately.

11 11. Colors schemes that advertise: Where color schemes are used that commonly identify  
12 the business on site, said areas shall be considered signage, and shall be included in the  
13 calculation of sign area. The fact that certain colors are "corporate" or "signature" colors  
14 shall not be grounds for waiver from this provision.

15 **Section 16.13 Utility Lines and Devices**

16 All new or relocated utility lines within a designated corridor shall be constructed and install  
17 beneath the surface of the ground unless it is determined by the Planning and Zoning Board that  
18 soil, topography, or any other compelling conditions make the installation of utility lines as  
19 prescribed herein unreasonable or impracticable.

20 A. It shall be the developer's responsibility to make the necessary arrangements with each utility  
21 in accordance with the utility's established policy.

22 B. The underground installation of incidental appurtenances such as transformer boxes, switch  
23 boxes, pedestal mounted boxes for the provision of electricity shall not be required. However,  
24 such appurtenances where not rendered impractical by the determination of the Planning and  
25 Zoning Board shall be installed on the site of any development approved after the adoption of  
26 this section. The necessary easements to allow the utility company access and service to  
27 such appurtenances shall be dedicated by the developer prior to issuance of a building permit.

28 C. All transformers, switch boxes, and utility devices related to development approved after the  
29 adoption of this Ordinance shall be set back a minimum of ten feet from the right-of-way and  
30 shall be screened appropriately so as not to be seen from any public right-of-way using  
31 landscape materials or construction in conformance with this chapter.

32 D. Nothing in this section shall be construed to prohibit any entity furnishing utility service within  
33 the town from collecting, as a condition precedent to the installation of service facilities, any  
34 fee, repayment, or contribution in aid of construction which may be required.

35 **Section 16.14 Walls and Fences**

36 A. All freestanding walls, sound barriers, ground sign enclosures, and the like fronting along the  
37 designated roadway or its major intersections shall be of brick. When a brick wall is used for a  
38 visual screen, the wall shall conform to the architectural style and the materials of the  
39 surrounding properties. Alternative materials and screens may be considered, if said  
40 alternatives are entirely in keeping with the spirit of this Ordinance, and such alternatives shall  
41 be reviewed by the Town's Appearance Review Board.

- 1 B. Fences visible from any right-of-way or private drive shall be decorative, yet functional, painted  
2 black or forest green, and shall be subject to the approval of the Town. Such fences shall be  
3 accompanied by a 3 foot high (within one year of planting) hedge surrounding the fence.  
4 Chain-link fences are prohibited in commercially zoned corridor areas. Chain link maybe  
5 considered in industrial areas when screened appropriately with berms and landscaping –  
6 subject to approval. Barbed wire fences are prohibited in all areas except industrial areas  
7 where said fencing is not viewable from the right-of-way.

8 **Section 16.15 Parking of Commercial and Recreational Vehicles**

9 All commercial parking and loading areas shall be designed and located so as to ensure visually  
10 appealing projects and to protect adjacent residentially zoned, used, or designated properties from  
11 any adverse impacts and noise.

12 A. Overnight parking of commercial vehicles, tractor trailers, boats, recreational vehicles,  
13 campers, or motor homes shall be prohibited within the Gateway Corridors, except in areas  
14 designated and designed for that purpose.

15 B. No commercial or personal vehicle with signage placed thereon shall be parked in a manner  
16 along a public street to be visible as an advertising device.

17 **Section 16.16 Screening of Mechanical Units**

18 Mechanical equipment and appurtenances such as air conditioner units, ventilation equipment,  
19 refrigeration systems, heating units, back-flow preventers, and satellite dishes must be screened  
20 so that they are not visible from any public right-of-way. The screen shall consist of a solid wall,  
21 façade, parapet or other similar screening material which is architecturally compatible and  
22 consistent with the associated building. If landscaping is utilized, the plantings must be high  
23 enough within one year of planting to provide a screen with a minimum of 75% opacity.

24 **Section 16.17 Storage/Service Areas; Miscellaneous**

25 All service areas shall be designed and located to be unobtrusive and architecturally integrated into  
26 the building's overall design.

27 A. Service, delivery, and dumpster areas must be located to minimize visibility from adjacent  
28 streets and adjacent properties. These areas shall be located at the rear or side of the building  
29 away from the right-of-way and be fully screened. Fence gates enclosing dumpster access  
30 points shall be opaque, not of wood construction, and shall be in good visual and structural  
31 condition.

32 B. No outside display of products of any kind, exclusive of the following, shall be permitted unless  
33 located in a fully screened enclosure: new and used motor vehicles (including boats, R.V.'s,  
34 motorcycles, personal watercraft, etc.), plant nurseries, and lumber yards. Outside display of  
35 the aforementioned vehicles must be on a well-maintained impervious surface. No more than  
36 50% of the vehicles displayed may be forward of the front building line.

37 C. No outside freestanding vending machines, video games, newspaper boxes, propane stations,  
38 electric rides, or similar devices shall be permitted unless fully screened from view from the  
39 adjacent roads and parking areas.

40 D. No outside work areas shall be permitted unless fully screened from view.

1 E. No outside shopping cart corrals shall be permitted adjoining a building or within parking areas  
 2 unless screened with walls and landscaping, or other screening materials deemed appropriate  
 3 by the Development Review Committee.

4 **Sec. 16.18 External Site Lighting**

5 Commercial buildings and projects, including their outparcels, shall be designed to provide safe,  
 6 convenient and efficient lighting for pedestrians and vehicles. Lighting shall be designed in a  
 7 consistent and coordinated manner for the entire project. Lighting shall be used to accent key  
 8 architectural elements and/or to emphasize landscape features, and shall be designed and  
 9 installed to avoid the creation of hot spots, glare or a nuisance.

10

11 A. General Lighting Standards

12 Light fixtures shall be designed as an integral design element that complements the design of the  
 13 project through style, material or color. All light poles and fixtures shall be black, dark green or  
 14 some similarly dark color that is consistent with the architectural design scheme of the property.  
 15 Lighting of on-site buildings shall be limited to wall-washer type fixtures or up-lights, which do not  
 16 produce spillover lighting or glare. Site lighting shall not incorporate floodlight fixtures mounted on  
 17 building walls, roofs, or poles.

18

19 B. Lighting Plan and Standards

20 To provide cohesiveness and uniformity, a lighting plan prepared by an architect or engineer  
 21 licensed to practice in the state of Florida shall be submitted to the Town as part of an application  
 22 for site plan approval, and said professional shall: (1) certify that the lighting plans are compliant  
 23 with the requirements of the design standards and regulations provided for herein; or (2) so certify  
 24 to the extent said lighting plans are compliant with the requirements of the design standards and  
 25 regulations provided for herein and provide a written explanation for any deviations. (3) provide a  
 26 photometric plan prepared by an engineer licensed and/or architect to practice in the State of  
 27 Florida.

28 The following level(s) of permitted foot-candles are required for all development, excluding single-  
 29 family residential parcels:

Location	Illumination level foot candle (f.c) maximums
All the lighting level(s) at the property line when adjacent to commercial/industrial uses	1 f.c.
All lighting level(s), at the property line, when adjacent to residential	0.5 f.c
Canopied Area (service stations, convenience centers, lighting under awnings, porte-cocheres, etc)	20 f.c.
Automatic Teller Machines (ATMs)	10 f.c. within 5 feet or 2 f.c. within 50 feet
Dealership (first aisle adjacent to the street)	24 f.c.

Dealership (remaining aisles)	10 f.c.
-------------------------------	---------

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25  
26  
27  
28  
29  
30  
31  
32  
33  
34  
35  
36  
37  
38

C. Design Features of Lighting:

1. Height. A light fixture (the pole and light source/luminary) shall be a maximum of thirty feet (30') in height within any parking lot, and a maximum of sixteen feet 16' in height within any non-vehicular pedestrian area (with height being measured from the finished grade to the top of the light fixture).
2. At service stations and convenience centers, lighting under awnings, canopies, porte-cocheres, etcetera, should be recessed. If not recessed, the box type or other lighting fixture shall be opaque on all sides (no light shall emanate from any side of the fixture).
3. To keep light rays and glare from encroaching onto adjacent properties, illumination shall be installed with house-side shields and reflectors, and shall be maintained in such a manner as to confine light rays to the premises.

D. Technical Deviations.

1. Any proposal which includes technical deviations from these **lighting** standards shall demonstrate the unique aesthetic and/or engineering design that meets or is within the spirit of these regulations. Such presentation shall include appropriate calculations and drawings or illustrations as necessary to explain the request or as may be required by the Town.
2. The Town Manager, or designee, shall make a determination whether to accept such proposed technical deviation after consulting with a mutually acceptable licensed professional engineer. The cost of making such determination shall be borne by the party requesting the technical deviation.

**Section 16.19 Variances/Special Exceptions**

Developments within Industrial zoning districts may request relief from certain provisions of this ordinance, if the applicant can demonstrate that the stated requirements would cause an undue hardship that merits special exception. Applications for special exceptions shall be determined through a consideration of the specific location of the property within the Gateway Corridor and adjacent uses. Such applications will be granted only if the proposed use complements or coordinates with adjacent uses, diversifies the economic base of the area, and comports with the intent of the Gateway Corridor ordinance.

**Section 16.20 Penalties**

- A. No building permit shall be issued until all provisions of this chapter are met.
- B. A Certificate of Occupancy shall not be issued for any structures until final approval by Town. This approval to include all provisions of this chapter.

**Section 16.21 Architectural Style**

The Town Commission shall adopt by resolution specific requirements concerning architectural

- 1 style of structures and amenities to be applicable in the Town Gateway Corridors.
- 2